

Solely for information purposes



Rijkswaterstaat
*Ministry of Infrastructure and the
Environment*



Blankenburgverbinding: contract & tender procedure

Yke Norg

Contract manager
Blankenburgverbinding

Industry day
Blankenburgverbinding & A16
Rotterdam

May 18th, 2016



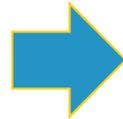
Project scope - overview

Availability:

- Existing infrastructure during construction
- All infrastructure 20 years after construction

Before:

A15, A20, greenfield



After:

A15 connected with A20 by a new A24

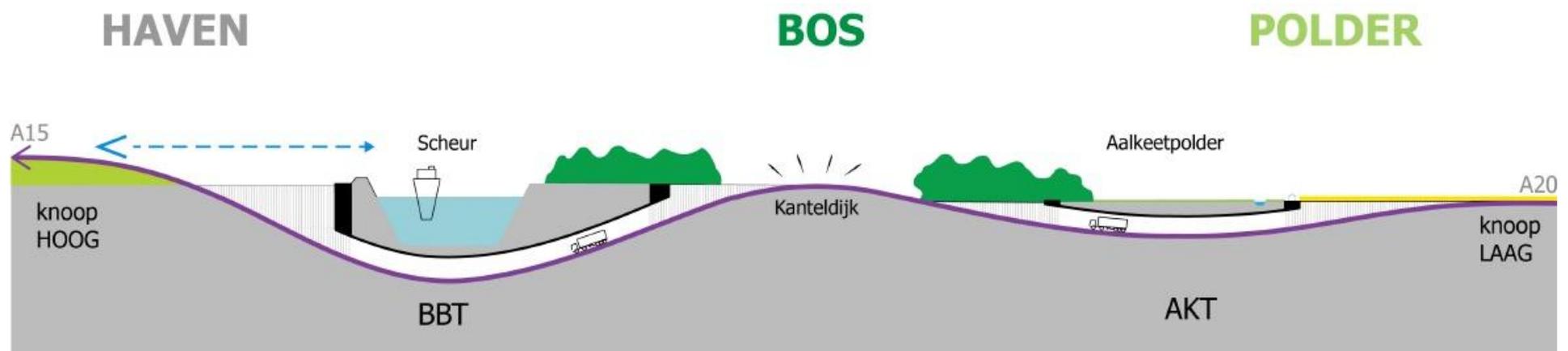
- Two new junctions: A15/A24 & A20/A24
- 2 x 3 lanes
- Land tunnel: Aalkeettunnel
- Immersed tunnel: Blankenburgtunnel



Main challenges (1/3)

Two tunnels (> 500m)

- Immersed tunnel under Scheur - Blankenburgtunnel (BBT)
- Land tunnel in Aalkeetpolder - Aalkeettunnel (AKT)
- Tunnels need to function as one system.





Main challenges (2/3)

Interfaces:

- Waterways (het Scheur)
- Flood protection (Kanteldijk)
- Roads
- Railways (Hoeksche Lijn: Rotterdam-Hoek van Holland)
- Cables and pipelines

Traffic management:

- Traffic Center Rhoon

Other contract:

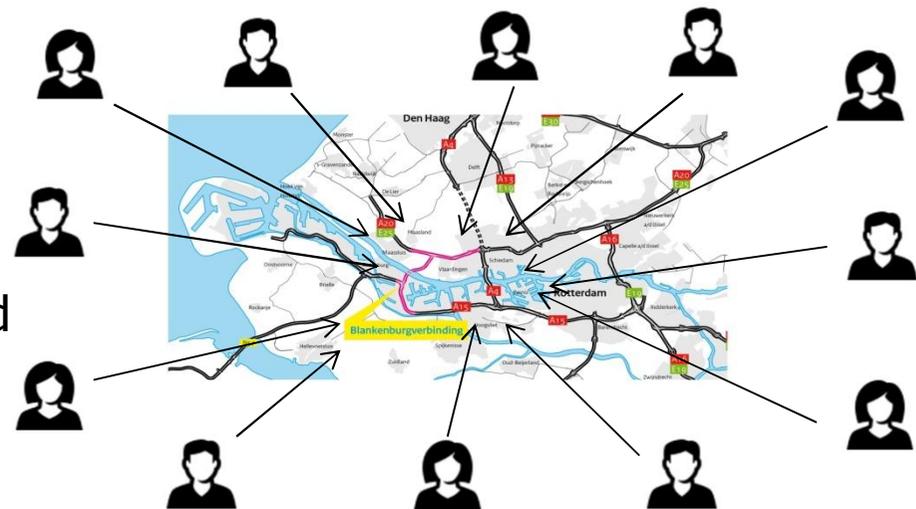
- DBFM A15 Maasvlakte-Vaanplein



Main challenges (3/3)

Stakeholder involvement

- Rijkswaterstaat West-Nederland Zuid
- Havenbedrijf Rotterdam
- Provincie Zuid-Holland
- Gemeente Rotterdam
- Gemeente Vlaardingen
- Gemeente Maassluis
- Hoogheemraadschap van Delfland
- Waterschap Hollandse Delta
- ProRail
- RET





Contract – starting points

Use of standards:

- Standard DBFM contract 4.3
- LTS (National Tunnel Standard) 1.2

With the following characteristics:

- construction period & maintenance of new & existing connections
- 20 years maintenance after construction
- Using lessons learned in DBFM-contract A9 Gaasperdammerweg (tunnel / LTS) & A6 Almere (simplified)

Professional project management by contractor:

- management specifications
- proof of meeting performance criteria through a Performance Measuring System



Contract – focus on performance

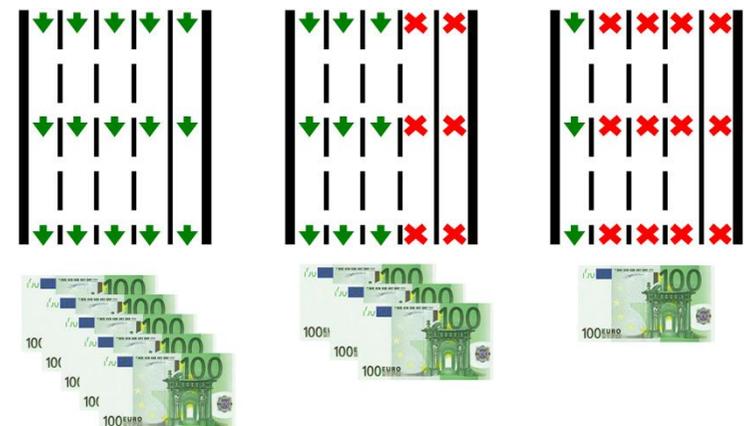
The required performance is **availability**:

- possible unobstructed and safe use, of all traffic lanes in all directions, for the road user

Performance regime principles:

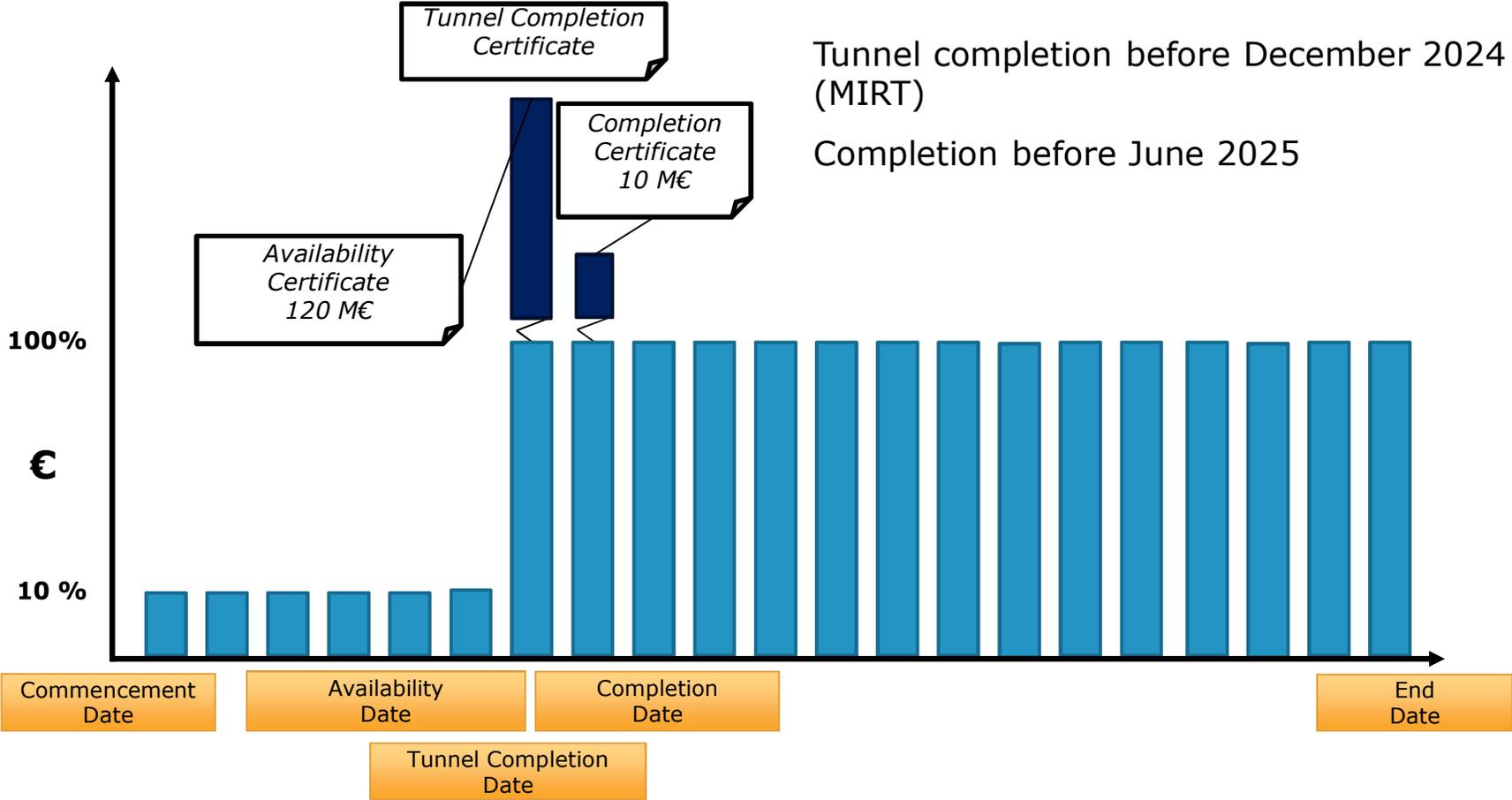
- aligns payments with performance delivered
- leads to predictable future cash flows
- incentivizes

Note: toll risk (volume) is not in scope DBFM-contract





Contract – payment characteristics





Toll

- Toll risk not in DBFM-contract (Availability Based Contract);
- Building some physical systems (portals)
- Coordination with toll contractor



Critical succes factors – timely opening (1/2)

- Opening before December 2024 (MIRT)
- Completion before June 2025

Required tunnel products

- Integral System Design Review (SDR)
- Integral Preliminary Design Review (PDR)
- Integral Critical Design Review (CDR)



Critical succes factors – timely opening (2/2)

Contractor is responsible for:

- Designing, building and testing two verified “safe” tunnels
 - **Two** tunnels need to function as **one system**.
- Obtaining the building permits (“Omgevingsvergunningen”)
- Documentation for the tunnel opening permit (“Openstellingsvergunning”)

Rijkswaterstaat is responsible for:

- Obtaining the tunnel opening permit (“Openstellingsvergunning”)



Critical succes factors – stakeholder management (1/2)

Communication: no surprises, informing and involving

Minimising disturbance

- Het Scheur (minimising obstructions, reliable planning, etc.)
- Stakeholders (residents and companies)
- Road users

Availability and accessibility during construction due to:

- long construction period
- large impact on main and secondary road network



Critical succes factors – stakeholder management (2/2)

Contractor is responsible for:

- obtaining all necessary permits (except the tunnel opening permit)
- fulfilling obligations of the Authority towards stakeholders (UVO)
- Following the principles of issue and stakeholder management (“strategisch omgevingsmanagement”)



'Marktvisie' – from ambition to implementation (1/2)

AMBITION	IMPLEMENTATION
Cooperation based on equality and complementarity – each with their own roles and responsibilities. The challenge is key.	Project result is key: best-for-project. Cooperation between Authority and Contractor is key.
Realising and connecting challenges	<ul style="list-style-type: none">• Learning from previous projects• Intensive cooperation between projects A16R and BBV• Tenders are in line with each other
Think, work, act and learn in value chains	Early involvement of stakeholders
Having a dialogue and anticipatory action	Open dialogue focused on <ul style="list-style-type: none">• Respect for each others interests/concerns• risks/design matters on the table• Realising a good cooperation• Realising the project (best for project) Discuss issues timely, honour commitments



'Marktvisie' – from ambition to implementation (2/2)

AMBITION	IMPLEMENTATION
To excell in work from realistic preconditions	<ul style="list-style-type: none">• Realistic planning (timing of projects BBV and A16R), stable scope, keep changes to a minimum• Providing insight in status of existing infrastructure;• Reduce transaction costs and apply convergence phase
Early dialogue about risks, information and dilemma's	<ul style="list-style-type: none">• Risks/design matters on the table• Discuss issues timely
Acting on a basis of strength and management by attitude and behavior	<ul style="list-style-type: none">• Training and good preparation,• Tendering a good contract,• identifying attitude and behaviour• Reflection
Competitive advantage by speed of applying the right knowledge	<ul style="list-style-type: none">• Transparent in sharing information about progress, worries and risks, no hidden agenda's,• Setting the right bidding criteria (disturbance, environmental impact, risk management)



Tender procedure

Competitive dialogue

- In two phases
- Including convergence phase for tunnel (simplified)

Expected duration: approximately 16 months between first phase dialogue and financial close

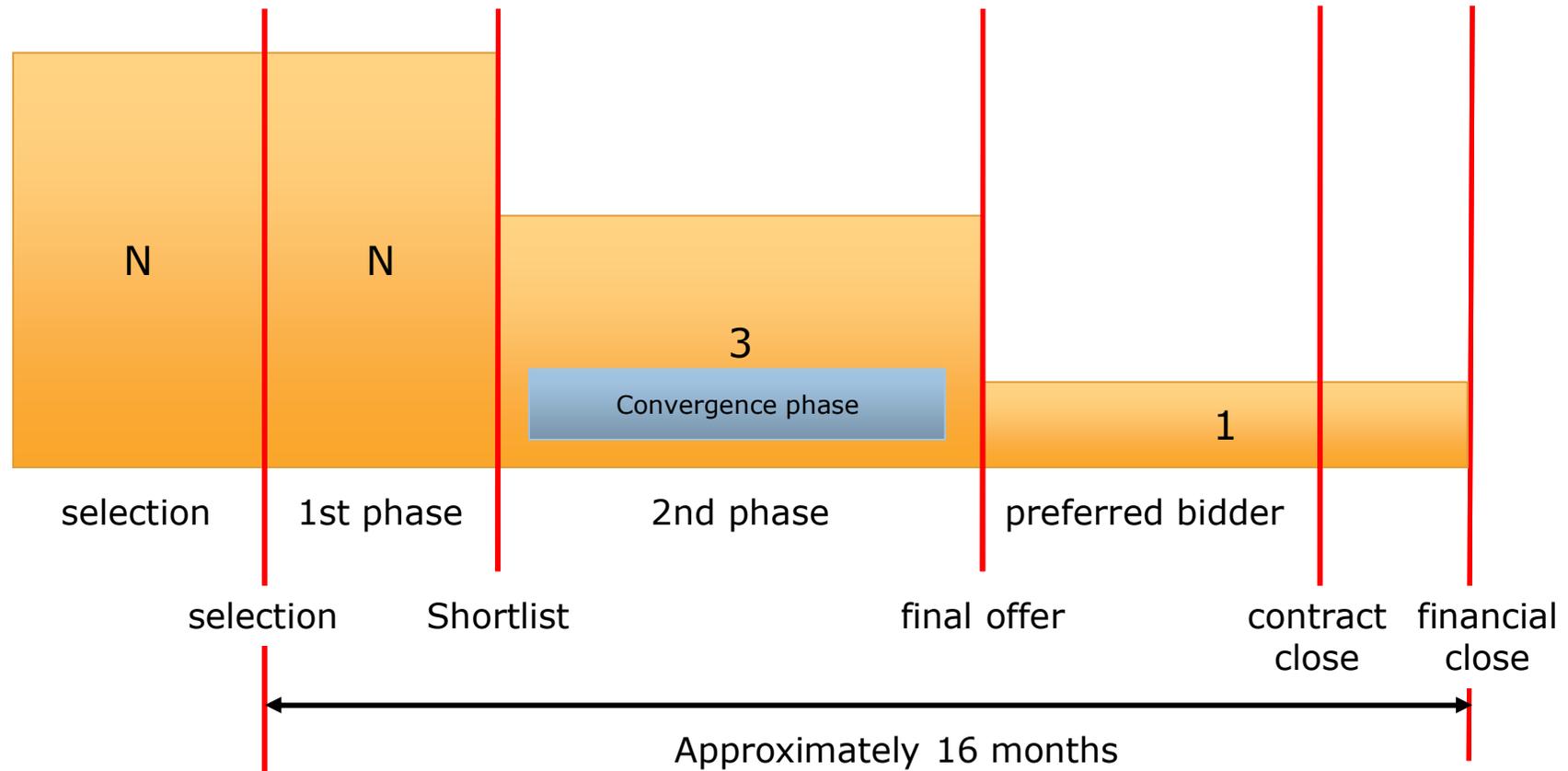
Contract and dialogue language Dutch

- contract and tender guidelines available in English for information only
- Dutch documents are legally binding

Note: digital signature for TenderNed!



Tender - Planning





Tender - Indicative time line

Announcement of the project on TenderNed	May 19th
Selection: Receive Letters of Interest	20-06-2016
Start Competitive Dialogue phase 1	14-07-2016
Final offer	Q2 2017
Contract and financial close	Q4 2017

Note: digital signature for TenderNed!



Tender - selection

Selection criteria:

- project management experience
 - experience with realizing a complete tunnel (incl. TTI, IA) that has been opened > 50 M€
- project finance experience > 50 M€
- capability for providing sufficient financing > 50 M€

Standard exclusion grounds

No shortlist during selection



Tender – First phase: shortlisting

3 authorities risks:

1. Port Authority does not grant a permit on 'het Scheur
2. A Contracting Authority Change because the Integrated Environmental Permit is granted under a condition that requires a change to the Agreement;
3. A decision by the competent administrative body or competent court, following an application, objection or appeal from a third party, either as an interim injunction or otherwise, to revoke, suspend or quash a permit to be granted to the Contractor

1 opportunity for realizing the project Blankenburgverbinding objectives (related to Stakeholder Management)

A shortlist of 3 bidders will be invited for the next phase of the dialogue



Tender – Second phase

Discuss bidder specific solutions

Share our technical project knowledge with bidders

Discuss integrated tunnel solutions (Convergence phase Tunnel (simplified))

- to eliminate differences in contract interpretation regarding tunnel specifications

Discuss draft products before submission as part of final offer



Tender – Bidding and validity criteria

Submit final offer under ceiling price

- Bid bond (5 M€) due at financial close

Validity criteria:

- Basic Planning including phasing plan
- Basic management plan
- Convergence phase tunnel
- Architectural design
- Approach to construction of Zuidoever
- Financial robustness:
 - funding plan
 - financial model
 - Cost breakdown & motivation



Tender – award criteria for bidding (1/3)

Economically Most Advantageous Tender:

Quantitative aspect:

- Price

Qualitative aspects:

- Environmental impact (DuboCalc, CO2-prestatieladder*) > 50 M€
- Stakeholder management (Stakeholder management, Limiting traffic congestion, disturbance) 70 M€
- Risk management capabilities 185 M€

* +1-5% of neutralised present value



Tender – award criteria for bidding (2/3)

	Authority's risks
1	Cables and pipelines
2	Tunnel Opening Permit has not been granted within 20 weeks after the Availability Date;
3	A Contracting Authority Change because the competent administrative body grants the Tunnel Opening Permit under a condition that requires a change to the Agreement
4	A Contracting Authority Change
5	Train Free Periods have not been granted



Tender – award criteria for bidding (3/3)

Authority's risks	
6	The Contracting Authority has not granted access to the relevant plot
7	DHMR doesn't grant a permit on 'het Scheur'
8	<ol style="list-style-type: none">1. A Contracting Authority Change because the comments of the Tunnel Manager make this necessary or because the competent administrative body grants the Integrated Environmental Permit for the Tunnel under a condition that requires a change to the Agreement;2. A decision by the competent administrative body or competent court, following an application, objection or appeal from a third party, either as an interim injunction or otherwise, to revoke, suspend or quash a permit to be granted to the Contractor



Solely for information purposes

Tender – Contract & Financial Close

Contract Close

Financial Close

- Bid bond (5 M€) due at Financial Close



Solely for information purposes

Communication during & after dialogue

Open and transparent communication (**cooperation**)

With respect for each other concerns and responsibilities

Confidential information will be treated as such

Focus on the project, risks and restrictive frameworks

Following the new 'Marktvisie'

This starts today!